

HIGHWAYS, TRANSPORT AND ENVIRONMENT OVERVIEW AND SCRUTINY PANEL

THURSDAY, 20TH JULY, 2017

At 6.30 pm

in the

MAY ROOM - TOWN HALL,

SUPPLEMENTARY AGENDA

PART I

<u>ITEM</u>	<u>SUBJECT</u>	<u>PAGE NO</u>
5.	<u>PARKING PROVISION</u> To consider the above report.	3 - 10
6.	<u>BROADWAY CAR PARK</u> To consider the above report.	11 - 62

This page is intentionally left blank

Report Title:	Borough Parking Plan
Contains Confidential or Exempt Information?	Yes - Appendix B Part II Not for publication by virtue of paragraph 3 of Part 1 of Schedule 12A of the Local Government Act 1972.
Member reporting:	Councillor Cox, Lead Member for Environmental Services (including parking) Councillor Evans, Lead Member for Maidenhead Regeneration and Maidenhead
Meeting and Date:	Cabinet Regeneration Sub-Committee 25 July 2017
Responsible Officer(s):	Russell O'Keefe, Executive Director
Wards affected:	All

www.rbwm.gov.uk



REPORT SUMMARY

- 1 The report sets out the emerging Parking Plan for the Borough based on the further assessments and feasibility studies that have been carried out which is fully consistent with the projections for need for parking in the Borough
- 2 The plan also re-confirms the overriding principle that parking needed to support new residential development will be provided as part of each these developments.
- 3 Cabinet Regeneration Sub-committee is asked to approve the emerging plan and next steps.

1 DETAILS OF RECOMMENDATION(S)

RECOMMENDATION: That Cabinet Regeneration Sub-Committee notes the report and:

- i) Approves the emerging Parking Plan and next steps.

2 REASON(S) FOR RECOMMENDATION(S) AND OPTIONS CONSIDERED

- 2.1 The regeneration programme, improved economic development opportunities, improved connectivity, and the borough's status as a major tourism destination does and will continue to generate an increase in parking demand across the borough.
- 2.2 In January 2017 Cabinet Regeneration Sub-committee considered a report that:
 - Showed the work had been undertaken taking account of all those areas in point 2.1 to analyse and better understand future parking need against current parking capacity.
 - Set out a parking model that tracks the relationship between provision and demand. This showed that parking provision in Maidenhead and Windsor are at

capacity and additional provision is required, particularly when taking into account the impact of regeneration in Maidenhead where car parking assets within the town centre will be temporarily removed and redeveloped

- Provided a draft Parking Plan which would fully meet need in the short and longer term with advice from parking consultants.
- Authorised the completion of detailed feasibility assessments for the potential sites for additional parking provision.

2.3 The feasibility work has been carried out by the Council's expert advisers Parking Matters. Discussions have also been held with a number of key stakeholders including the Council's Joint Venture Partner, Countryside Ltd.

2.4 The parking plan has now been further refined and the updated version is set out in Appendix A.

2.5 A headline summary of the additional parking arrangements in the emerging plan is set out in the table below.

Location	Public Parking	Spaces	Available
Current Proposed Temporary Solutions			
Maidenhead	Reform Road – Temporary decks to be used as Council staff parking	300	Quarter 3 2018/19
Maidenhead	Hines Meadow – Additional capacity by freeing up of 250 Council staff spaces and change in use of spare capacity to support the town centre.	450	Quarter 3 2018/19
Maidenhead	St Clouds Way – Temporary decked solution until permanent arrangements completed to be built on the current tenpin bowling area following demolition.	600	Quarter 3 2018/19
Proposed Permanent Solutions			
Maidenhead	St Clouds Way – Underground car parking	260	Quarter 2 2023
Maidenhead	Braywick Park – additional car parking	200 initially rising to 500	Quarter 3 2018 and Quarter 2 2019
Maidenhead	Stafferton Way – additional deck	125	Quarter 3 2018/19
Maidenhead	Broadway Car Park	1,500	Quarter 4 2019/20
Windsor	River Street	113	

2.6 Discussions are continuing with a number of other parties on the proposals above and other options and through that work the final proposals may be refined further, with the

final investment case coming to Full Council for approval in November 2017 alongside the investment case for Broadway Car Park which is a separate report on this agenda.

- 2.7 The principle applied throughout has been that the final implementation of the parking plan will ensure current unmet parking demand and need and the impact of future development and regeneration plans is fully met.
- 2.8 The emerging parking plan also confirms another overriding principle that parking needed to support new residential development will be provided as part of each these developments. For example, there are over 4,000 new residential units planned for Maidenhead Town Centre and Maidenhead Golf Club and surrounding land and parking provision for these will be built into these schemes.
- 2.9 There are also potential private projects that the Council is aware have been explored that may go ahead and deliver new parking provision in Maidenhead of up to 950 spaces.
- 2.10 Analysis has shown that the mitigation of the parking pressures and need in Maidenhead is not reliant on this private provision subject to all of the options within the plan being implemented. This provision will however act as contingency should detailed feasibility of any of the other options suggest they are not viable.

Maidenhead

- 2.11 The new parking arrangements in the emerging plan utilise temporary decking solutions to support the regeneration of Maidenhead which will see some existing parking sites redeveloped:
- Reform Road – A 300 space decked solution would be installed at Waldeck House and used for Council staff car parking which will free up 250 spaces in Hines Meadow Car Park to support public parking in the town centre whilst the Broadway Car Park is redeveloped. A further 200 spaces of spare capacity in this car park will also be available for this purpose.
 - St Clouds Way – As part of the wider development of the site, the tenpin bowling facility will be demolished and a 600 space decked solution would be installed to support town centre parking.
- 2.12 Alongside this temporary provision there would be significant investment in new permanent parking provision:
- Broadway Car Park (often referred to as Nicholson's) – this would see the creation of a much larger circa 1,500 space, state of the art, town centre car park which is set out in detail in a separate report on the agenda. Between 225 and 500 of these spaces would be utilised to support the proposed Landing Development.
 - Stafferton Way Car park – An additional 125 space permanent level would be installed at the Stafferton Way multi storey car park to support long stay car parking.
 - St Clouds Way – As part of the wider development of the site a new 260 space permanent underground public car park will be developed through the Council's Joint Venture with Countryside Ltd.

- Braywick Park – As part of the development of the new leisure centre 200 spaces will be developed initially as surface car parking rising to 500 spaces when the leisure centre is completed to support the new centre and long stay car parking in the Town.

Windsor

- 2.13 Analysis of parking demand and provision in Windsor has highlighted that there is sufficient demand to support the implementation of an additional deck at River Street car park which would be implemented as part of the emerging plan.
- 2.14 Work will also be taken forward to create new residential parking provision. This would not generate income based on current resident permitting arrangements and have not been included within the Parking Plan. They will be reported as discrete options.

Ascot and the Sunnings

- 2.15 The work has not shown a case for an additional public car park but work will now be taken forward to review resident permitting to improve the management of parking in the area.

Next steps

- 2.16 To continue discussions with other parties and stakeholders to finalise the proposals in the emerging plan and propose a final investment case to Full Council for approval in November 2017 alongside the investment case for Broadway Car Park which would then be implemented.

Table 3: Parking Provision Options Considered

Option	Comments
Agree the updated Parking Plan The recommended option	This option will facilitate complete mitigation of the future parking pressures and demands within the Royal Borough.
Reject the updated Parking Plan Not recommended	Parking provision in both Maidenhead and Windsor is at capacity. Failure to provide further parking provision may result in parking being displaced onto residential roads and/or people choosing not to visit the Royal Borough due to poor parking provision.

3 KEY IMPLICATIONS

Table 2: Key implications

Outcome	Unmet	Met	Exceeded	Significantly Exceeded	Date of delivery
Parking Plan implemented to	Not implemented in line with	All arrangements in place in	All arrangements in place	All arrangements in place	Quarter 4 2019/20

Outcome	Unmet	Met	Exceeded	Significantly Exceeded	Date of delivery
timetable	plan	line with plan	ahead of schedule	ahead of schedule and under budget	

4 FINANCIAL DETAILS / VALUE FOR MONEY

- 4.1 The next steps can be completed within existing budgets with the final investment case coming to Full Council for approval in November 2017.

Table 3: Financial impact of report's recommendations

REVENUE	2017/18	2018/19	2019/20	2020/21
Addition	£0	£0	£0	£0
Reduction(income)	£0	£0	£0	£0
Net impact	£0	£0	£0	£0

CAPITAL	2017/18	2018/19	2019/20
Addition	£0	£0	£0
Reduction	£0	£0	£0
Net impact	£0	£0	£0

5 LEGAL IMPLICATIONS

- 5.1 The procurement of contractors will be conducted in accordance with the Public Contracts Regulations 2015.

6 RISK MANAGEMENT

Table 4: Impact of risk and mitigation

Risks	Uncontrolled Risk	Controls	Controlled Risk
The contractors do not have the necessary skills to progress the project	High	Robust specification and procurement process	Low
The projects exceed the cost envelope or planned	High	Effective development management processes	Low

Risks	Uncontrolled Risk	Controls	Controlled Risk
timescales			

7 POTENTIAL IMPACTS

- 7.1 An Equality Impact Assessment Initial Screening has been undertaken and concludes the proposals contained in this report will not unlawfully discriminate against any group or individual, or provide the grounds for such discrimination.

8 CONSULTATION

- 8.1 The report will be considered by Corporate Services Overview and Scrutiny Committee.

9 TIMETABLE FOR IMPLEMENTATION

Table 5: Implementation timetable

Date	Details
Nov 17	Full Council approval of Investment Case
Dec'17 – Mar '18	Planning period & procurement of temporary solutions
April'18 – May '18	Period prior to works commencing on site
June'18 – July '18	Works to existing sites/groundworks etc.
August'18 – Jan '19	Assembly works
Nov 18 – Feb 19	Temporary parking structures operational

- 9.1 Implementation date if not called in: Immediately

10 APPENDICES

- 10.1 Appendix A: - Parking Plan
Appendix B – Feasibility Study – Part II

11 BACKGROUND DOCUMENTS

- 11.1 None

12 CONSULTATION (MANDATORY)

Name of consultee	Post held	Date sent	Comment ed & returned
Cllr Carwyn Cox	Lead Member for Environmental Services (including parking)	13/06/2017	
Cllr David Evans	Lead Member for	13/06/2017	

Name of consultee	Post held	Date sent	Comment ed & returned
	Regeneration and Maidenhead		
Cllr Jack Rankin	Lead Member for Economic Development and Property	13/06/2017	
Cllr Philip Love	Deputy Lead Member for Maidenhead Regeneration	13/06/2017	
Alison Alexander	Managing Director	12/06/2017	
Russell O'Keefe	Executive Director	12/06/2017	
Andy Jeffs	Executive Director	12/06/2017	
Rob Stubbs	Section 151 Officer	12/06/2017	
Terry Baldwin	Head of HR	12/06/2017	
Mary Kilner	Head of Law and Governance	12/06/2017	
	Other e.g. external		

Appendix A - Parking Provision Plan		2017/18								2018/19										2019/20								2020/21				Comments		
		Quarter 1		Quarter 2		Quarter 3		Quarter 4		Quarter 1		Quarter 2		Quarter 3		Quarter 4		Quarter 1		Quarter 2		Quarter 3		Quarter 4		Quarter 1								
Maidenhead	Change				Re-provn			Loss			New			Re-provn		Loss	New		Loss			New		Loss	New			Loss		Loss	New			
Hines Meadow	Chapel Arches Phase 3 - LS			0				-155																										
	Unused Capacity Floor 4 - SS				100								100																					
	Transfer Council staff parking to Reform Rd																250																	
Reform Road	Temporary decks																249																	
	Transfer staff from HM															-250																		
	Release of temporary deck																												-249					
Braywick Park	Return of perm spaces										60																							
	New spaces - permanent																200																	
	New leisure centre																								240									
West Street	Regeneration															-59																		
	Reprovision by Joint Venture - St Clouds Way													0																				
Staff Way MSCP	Additional deck permanent																125																	
Town Hall	Regeneration																		-111															
	Reprovision by Joint Venture - St Clouds Way																																	
St Ives/Grove Rd	Regeneration																															83 spaces removed Q2 2023		
	Reprovision by Joint Venture - St Clouds Way																																	
Magnet /St Clouds Way	Loss																								-248									
	Developer replacement permanent																															260 spaces in 2023		
	Temporary decks																600											-600						
Nicholsons / Broadway	Closed for redevelopment																		-743															
	NEW MSCP																												1,000					
Maidenhead Station Forecourt	Regeneration															-82																		
	Reprovision -permanent deck at Stafferton Way																																	
STARTING		3,447		3,447			3,547		3,547		3,392			3,452			3,552			4,585			3,731			3,731			3,723		4,123			
Sub Total Loss		0		0			0		-155		0			0			-391			-854			0			-248				-600		-249		
Sub Total Reprovision					100									100																				
Sub Total Additional											60						1,424					0				240								
Sub Total Spare Capacity																																		
Demand			0			0		0				0			0			0		0			0			0				1,000		0		
Quarter's Impact		0	0			100	0	0		-155		60			100			1,033		-854		0				-8			-600	400		-249		
CLOSING			3,447			3,547		3,547		3,392			3,452			3,552			4,585		3,731			3,731			3,723			4,123		3,874		
	Cumulative capacity change					100		100		-55		5			105			1,138		284			284			276				676		427		

CP	Current Capacity	
Boulters Lock		87
Hines Meadow		1,280
Magnet		248
Nicholsons		734
Stafferton Way		576
Town Hall		111
West Street		59
Silco Drive (NR)		100
Station Forecourt		90
Braywick Park		80
Grove Road		82
Totals		3,447

Report Title:	Broadway Car Park
Contains Confidential or Exempt Information?	Yes – Appendix B and C Part II Not for publication by virtue of paragraph 3 of Part 1 of Schedule 12A of the Local Government Act 1972.
Member reporting:	<p>Councillor Evans, Lead Member for Maidenhead Regeneration and Maidenhead (including School Improvement)</p> <p>Councillor Cox, Lead Member for Environmental Services (including parking)</p> <p>Councillor Philip Love, Deputy Lead Member for Maidenhead Regeneration and Maidenhead</p>
Meeting and Date:	Cabinet Regeneration Sub-Committee 25 July 2017
Responsible Officer(s):	Russell O'Keefe, Executive Director
Wards affected:	All

www.rbwm.gov.uk



REPORT SUMMARY

- 1 The report sets out a proposed development brief for the redevelopment of the Broadway Car Park based on the feasibility study and financial modelling that has been carried out and requests approval to move to the next stage of design.
- 2 The redevelopment of the car park is a key part of Maidenhead town centre regeneration and will help to meet parking demand in the town and support the local economy.

1 DETAILS OF RECOMMENDATION(S)

RECOMMENDATION: That Cabinet Regeneration Sub-Committee notes the report and:

- i) Approves the development brief for the redevelopment of the Broadway Car park.
- ii) Delegate authority to the Executive Director in liaison with the Lead Member for Maidenhead Regeneration and Maidenhead (including school improvement), the Lead Member for Environmental Services (including parking) and the Deputy Lead Member for Maidenhead Regeneration and Maidenhead to appoint a professional team to complete the next stage of design.

2 REASON(S) FOR RECOMMENDATION(S) AND OPTIONS CONSIDERED

- 2.1 Broadway Car Park (often referred to as Nicholson's Car Park) forms part of the Broadway Opportunity Area detailed in the adopted Maidenhead Town Centre Area Action Plan (AAP). The car park is linked to the Nicholson's shopping centre and is the key town centre car park for shoppers.
- 2.2 The car park is reaching the end of its lifespan and is in need of significant repair and refurbishment. A replacement car park is essential to meet parking demand and the expected growth of the town centre.
- 2.3 A comprehensive redevelopment of the site would not only increase parking capacity but would also provide an opportunity to create high quality modern retail space on the ground floor, effectively extending the town's prime retail area back to Broadway and creating a strong retail frontage immediately opposite The Landing. The proposed layout will significantly improve pedestrian flows, from the Station, through the Landing, and the Nicholson's Centre, to the High Street, enabling the shopping centre, new retail and The Landing to attract residents, shoppers and visitors and function as an integrated, revitalised town centre.
- 2.4 The car park is currently unsightly and obstructs the High Street and shopping centre from the Station and The Landing. However, due to its central location has an important role to play as a focal point and facility for the town centre and helping in the future of the Nicholson shopping centre and also to complement and encourage the early delivery of the Landing as part of the larger town centre strategy.
- 2.5 The Council has a contract with Ryger (Maidenhead) Limited to deliver the car park redevelopment (or refurbishment) by the date that the Landing is completed. Ryger are committed to pay £2,500,000 to the Council to provide 225 parking spaces for use by the office occupiers of The Landing.
- 2.6 There has been an agreement in place with the Council since 9th March 2015 and a compulsory purchase process is commencing with a view to starting construction of the Landing within the next 12 -18 months when this process is complete.
- 2.7 A new, larger Broadway Car Park integrated with the shopping centre, High Street and The Landing and delivered as soon as possible is aligned with the towns and the Councils needs.
- 2.8 A full planning permission was originally obtained in October 2015 for a larger car park but it is not deemed either big enough or of sufficient merit to implement.
- 2.9 Various options have been considered to deliver a new car park including selling to (or partnering with) a private sector developer, or a joint venture with adjoining owners.
- 2.10 However, in October 2016 Cabinet Regeneration Sub-Committee agreed the principle that the Council progresses the option of developing the car park itself, as owner using its own funds potentially with another investor e.g. the Berkshire Pension Fund subject to approval of an investment case by Full Council.

- 2.11 The Council developing the car park itself will ensure this key part of the regeneration of Maidenhead is delivered, provide control over the process and result in the Council having a new income generating asset to replace the aging existing car park.
- 2.12 A feasibility study (appendix A) and cost modelling (appendix B) has been carried out by a temporary professional team that have been put together by the development manager's working on the project on behalf of the Council, the London and Aberdeen Group.
- 2.13 The feasibility study, cost modelling and development brief (Appendix C) show that a range of key features should be deliverable including:
- Increased capacity from the current circa 734 spaces to circa 1500 spaces (circa 1,435 in the new car park plus 100 in the adjoining Nicholson's car park) of which 50% will include electric charging facilities. It is expected that between 225 and 500 of these spaces will be utilised to support the proposed Landing Development.
 - Circa 11% disabled and parent and child spaces and new shopmobility facilities.
 - Generous bay sizes and column free parking.
 - Good circulation around the car park supported by electronic signage and safe pedestrian routes to improve user experience.
 - New disabled , drop off and retail delivery and service arrangements.
 - New circa 18,500 square foot of ground floor retail space to animate Broadway and link the shopping centre to The Landing and The Station t and attract quality retail to the Town to enhance the shopping offer.
 - Improved public realm along Broadway and King Street making it more pedestrian friendly.
 - A dynamic and visually interesting facade to the car park tailored to the setting which acts as a focal point building along Broadway.
- 2.14 Work has also been undertaken to develop the investment case for subsequent approval by Full Council. This work has been recently been supported by the professional services firm Grant Thornton. It is proposed that the Council now move to appoint a professional team to move the project to next stage of design which will allow the investment case to be further refined and a final version brought to Full Council for approval in November 2017.
- 2.15 The Council's purchase of Central House facilitates the development of the car park. A separate feasibility study will be carried out on Central House and report brought forward in due course with a proposed development option.

Table 1: Options

Option	Comments
To agree the proposed development brief and that the project moves to the next stage of design	This would ensure this key part of the regeneration of Maidenhead is delivered and result in the Council having a new income generating asset.
To not agree the proposed development brief and that the project moves to the next stage of design	This would mean this opportunity for the town and the Council would be delayed.

3 KEY IMPLICATIONS

Table 2: Key implications

Outcome	Unmet	Met	Exceeded	Significantly Exceeded	Date of delivery
Investment case agreed by Full Council and Planning application agreed by Cabinet Regeneration Sub-committee and submitted	Not developed	November 2017	October 2017	September 2017	30 November 2017

4 FINANCIAL DETAILS / VALUE FOR MONEY

- 4.1 The work to move to the next stage of design can be completed within the provision already within the Council's capital programme for this project.

Table 3: Financial impact of report's recommendations

REVENUE	2017/18	2018/19	2019/20
Addition	£0	£0	£0
Reduction	£0	£0	£0
Net impact	£0	£0	£0

CAPITAL			
Addition	£0	£0	£0
Reduction	£0	£0	£0
Net impact	£0	£0	£0

5 LEGAL IMPLICATIONS

- 5.1 The procurement of a professional team will be conducted in accordance with the Public Contracts Regulations 2015.

6 RISK MANAGEMENT

6.1 A detailed risk register is included within the development brief at Appendix C

Table 4: Impact of risk and mitigation

Risks	Uncontrolled Risk	Controls	Controlled Risk
The professional team do not have the necessary skills to progress the project	High	Robust specification and procurement process	Low
The project exceeds the cost envelope or planned timescales	High	Effective development management processes	Low

7 POTENTIAL IMPACTS

7.1 An Equality Impact Assessment Initial Screening has been undertaken and concludes the proposals contained in this report will not unlawfully discriminate against any group or individual, or provide the grounds for such discrimination.

8 CONSULTATION

8.1 The report will be considered by Corporate Services Overview and Scrutiny Committee.

9 TIMETABLE FOR IMPLEMENTATION

Table 5: Implementation timetable

Date	Details
July 2017	Complete appointment of full professional team
By November 2017	Concept design including pre-application discussions
November 2017	Council approval of the investment case and Cabinet regeneration Sub-Committee approval to progress to planning application submission.
February 2018	Completion of developed design
Late Spring 2018	Determination of planning application and commence construction tender process
Early 2019	Demolition of existing car park
Spring 2020	Practical completion of car park

9.1 Implementation date if not called in: Immediately

10 APPENDICES

10.1 Appendix A - Architectural Feasibility Study by Allies & Morrison

10.2 Appendix B – Construction Cost Report by Core Five – Part II

10.3 Appendix C – Development Brief - Part II

11 BACKGROUND DOCUMENTS

11.1 None

12 CONSULTATION (MANDATORY)

Name of consultee	Post held	Date sent	Commented & returned
Cllr David Evans	Lead Member for Regeneration and Maidenhead	12/7/2017	
Cllr Carwyn Cox	Lead Member for Environmental Services (including parking)	12/7/2017	
Cllr Jack Rankin	Lead Member for Economic Development and Property	12/7/2017	
Cllr Philip Love	Deputy Lead Member for Maidenhead Regeneration	12/7/2017	
Alison Alexander	Managing Director	12/6/2017	
Russell O'Keefe	Executive Director	12/7//2017	
Andy Jeffs	Executive Director	12///2017	
Rob Stubbs	Section 151 Officer	12/7/2017	
Terry Baldwin	Head of HR	12/7/2017	
Mary Kilner	Head of Law and Governance	12/7/2017	
	Other e.g. external		



NICHOLSON CAR PARK MAIDENHEAD

ARCHITECTURAL FEASIBILITY STUDY

DRAFT

MAY 2017
ALLIES AND MORRISON

1 INTRODUCTION

1.1 Introduction

2 KEY DESIGN PRINCIPLES

- 2.1 Capacity
- 2.2 Streetscape
- 2.3 Ground Uses
- 2.4 Street Pattern
- 2.5 Exemplary Design
- 2.6 Access and Movement
- 2.7 Central House
- 2.8 Facade

3 PROPOSAL

- 3.1 Site Plan
- 3.2 Lower Ground Floor Plan
- 3.3 Ground Floor Plan
- 3.4 First Floor Plan
- 3.5 Typical Upper Floor Plan
- 3.6 Site Section
- 3.7 Key Design Data
- 3.7 Access, Movement & Traffic Flows

4 MASSING STUDIES

5 FACADE PRECEDENTS

1. INTRODUCTION

This feasibility study has been prepared by Allies and Morrison to respond to the need to maximise the number of parking spaces at the Nicholson's Car Park for Maidenhead Town Centre.



2.1 CAPACITY

- Maximise the number of parking spaces for Maidenhead Town Centre
- Provision for c. 1,400 spaces
- Minimise, or offset, the short and medium term impact on Council revenue caused by demolition and redevelopment of the car park
- The project to be as deliverable and low risk as possible
- Ideally, continue to provide access to adjacent car parking spaces



2.2 STREETScape

- Introduce high quality street frontages to Broadway and King Street
- Ground the building through façade treatment at street level
- Improve Public realm along Broadway and King Street making it more pedestrian friendly
- Provide ease of connection to The Landing at street level



2.3 GROUND USES

- Deliver generous ground floor retail space to animate Broadway and Kings Street
- Provide retail space that complements and enhances the town centre shopping experience
- Provide drop off bay for those accessing shop mobility
- Provide ease of access and good connections for pedestrians from ground floor
- Provide delivery areas for the ground floor retail units



2.4 STREET PATTERN

- Contribute to a new connection between the station and the Landing with the Nicholson's Shopping Centre and high street beyond
- Establish a new public space at the corner of Broadway and Kings street with retail frontage
- Continue the shopping experience from King Street to Broadway and through to the Shopping centre
- A consistent new building line for the development, at the back of the pavement, to establish an appropriately urban setting for Broadway



2.5 EXEMPLARY DESIGN

- Generous, larger than minimum standard parking spaces
- Create safe pedestrian routes within car park
- Provide generous floor to soffit heights for improved user experience
- Allow natural light to penetrate deep into the interior
- Support a natural ventilation solution to the car park
- Well designed, accessible lift and stair cores
- Large span, open floor plates utilising standard car park construction systems



2.6 ACCESS AND MOVEMENT

- Dedicated access/ exit point served by a new junction on Broadway
- Entry/ exit barriers located at first floor level
- Dedicated Shop Mobility facilities
- Separate up/ down ramps
- Variable Message Signage system (VMS) for improved user experience



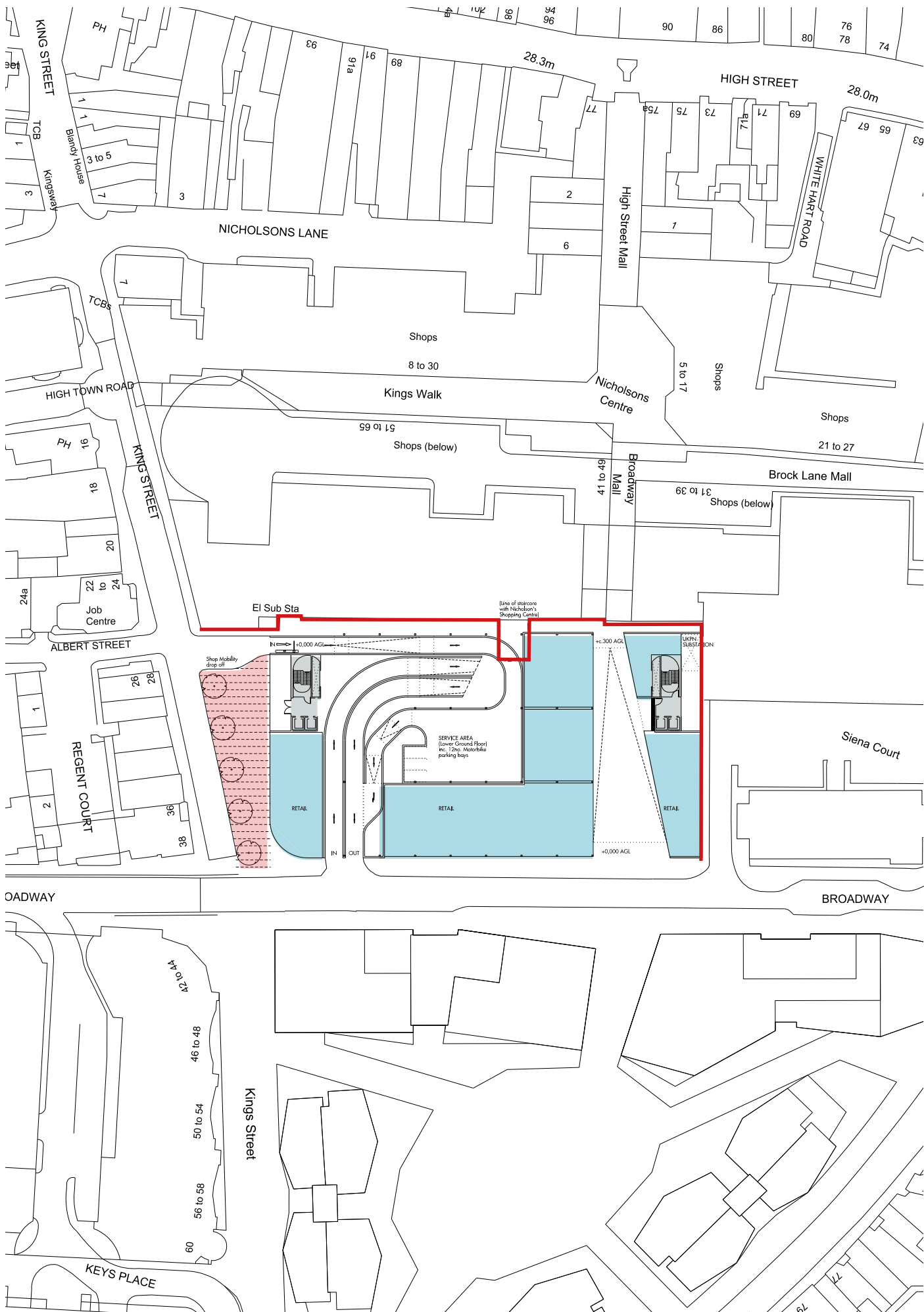
2.7 CENTRAL HOUSE

- Central House is subject to a separate opportunity study to consider remodelling, refurbishment and/ or demolition
- Significantly improving visual and architectural qualities of Central house to act as a complement to adjacent development
- Enable new life for Central House with a new range of uses
- A new address with enhanced entrance into Central House
- Positively contribute to the skyline and provide an improved frontier with the taller buildings of the town centre



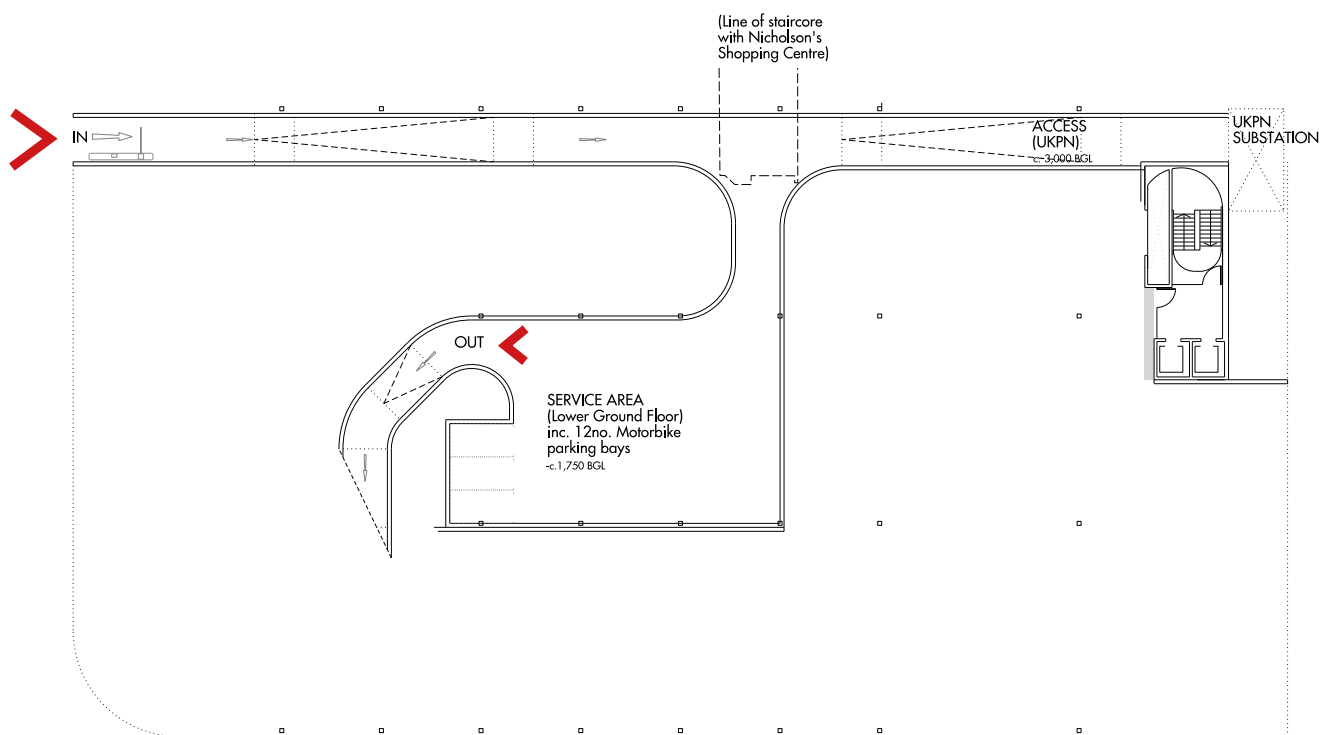
2.8 FACADE

- Creating a beautiful, dynamic and visually interesting facade to car park tailored to respond to setting
- Enhance the setting by providing a strong backdrop to adjacent buildings
- Create variety in façade at ground level
- A focal point along Broadway, the building will become a key marker between the railway station to the town centre
- Allow natural light to penetrate into the interior, support a natural ventilation solution and hide the structure behind a homogeneous surface



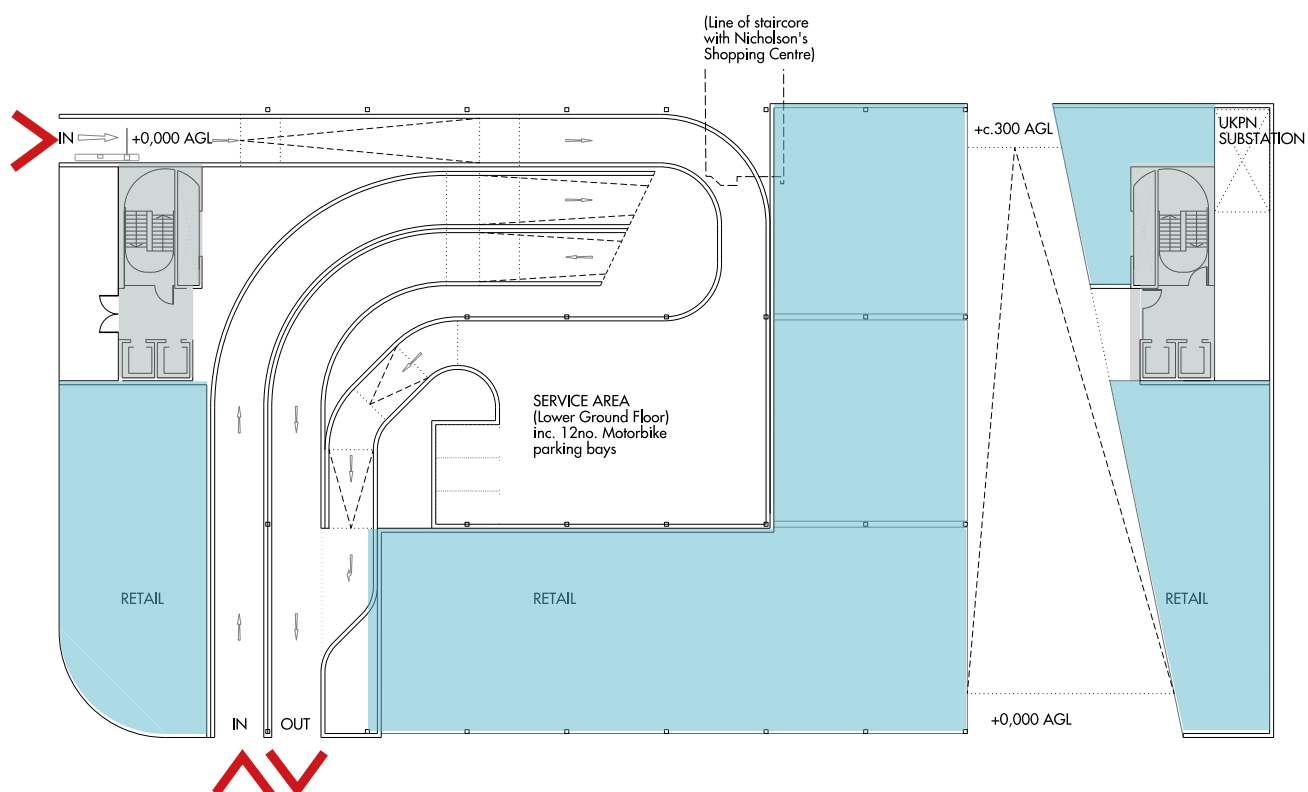
3 SITE PLAN

- Creation of new public square and drop off area to King Street
- Creation of generous retail connection from The Landing through to Nicholson's Shopping Centre
- Generous retail facades/street frontages to Broadway and King Street animating ground floor public realm level and grounding building
- Eastern Core accessible directly of new connection allowing ease of access between car park and shopping centre
- Western core allows ease of access from King street
- Delivery area provided for ground floor retail units
- Potential deliveries and shop mobility vans can access from King Street/drop off area
- Access can be provided to existing substation if required



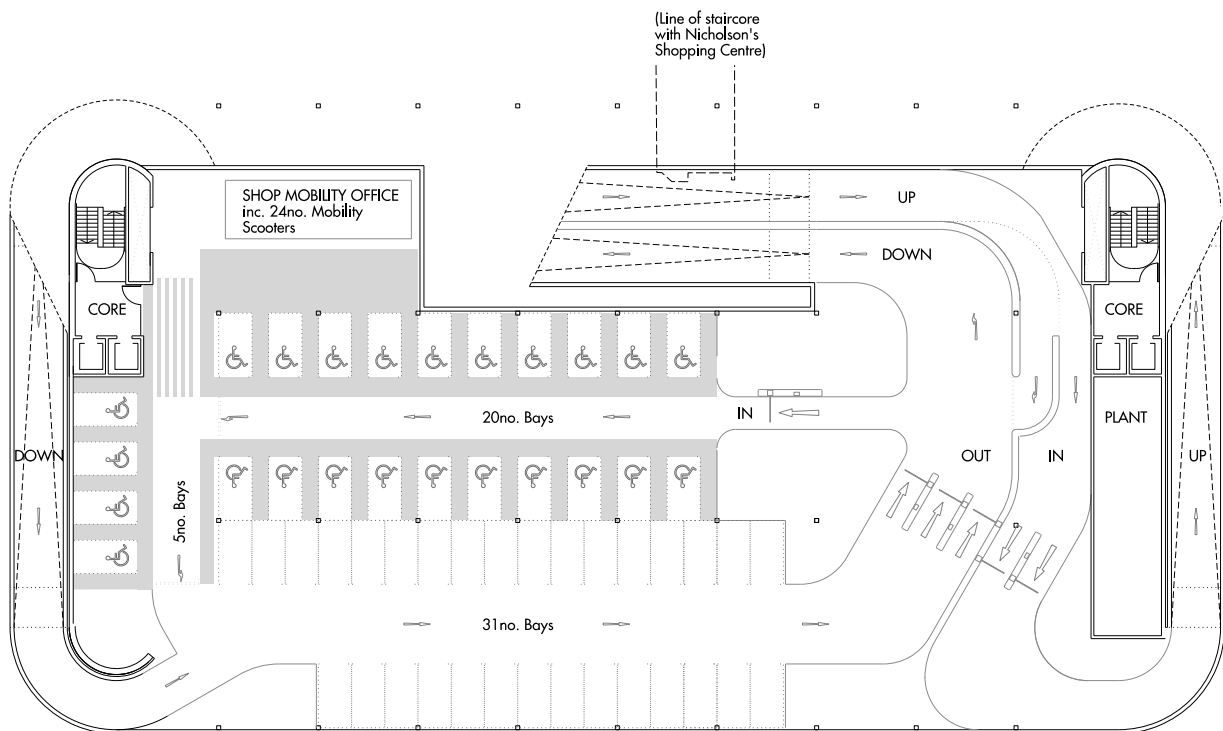
LOWER GROUND FLOOR PLAN

- Secure service area supporting retail deliveries, car park maintenance
- UKPN access to electrical substation



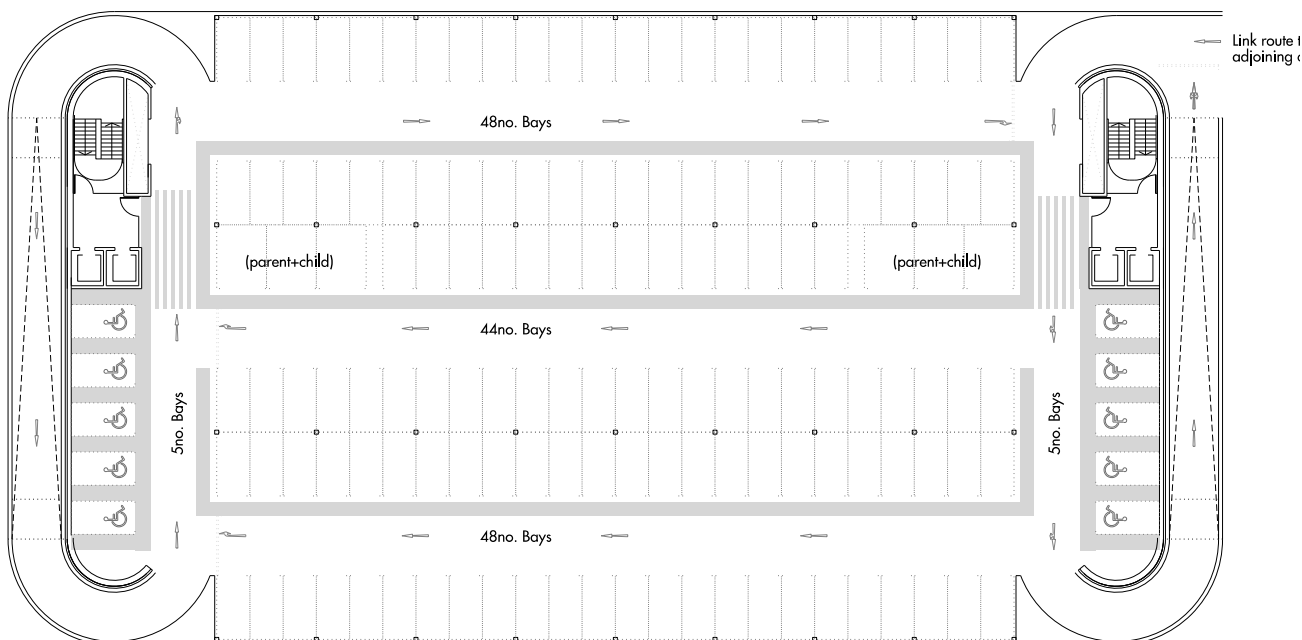
GROUND FLOOR PLAN

- c.18,700 sq.ft retail opportunity
- New pedestrian route connecting Nicholson's Shopping Centre with the Landing and Station beyond
- Service and delivery access via King Street to lower ground floor service area



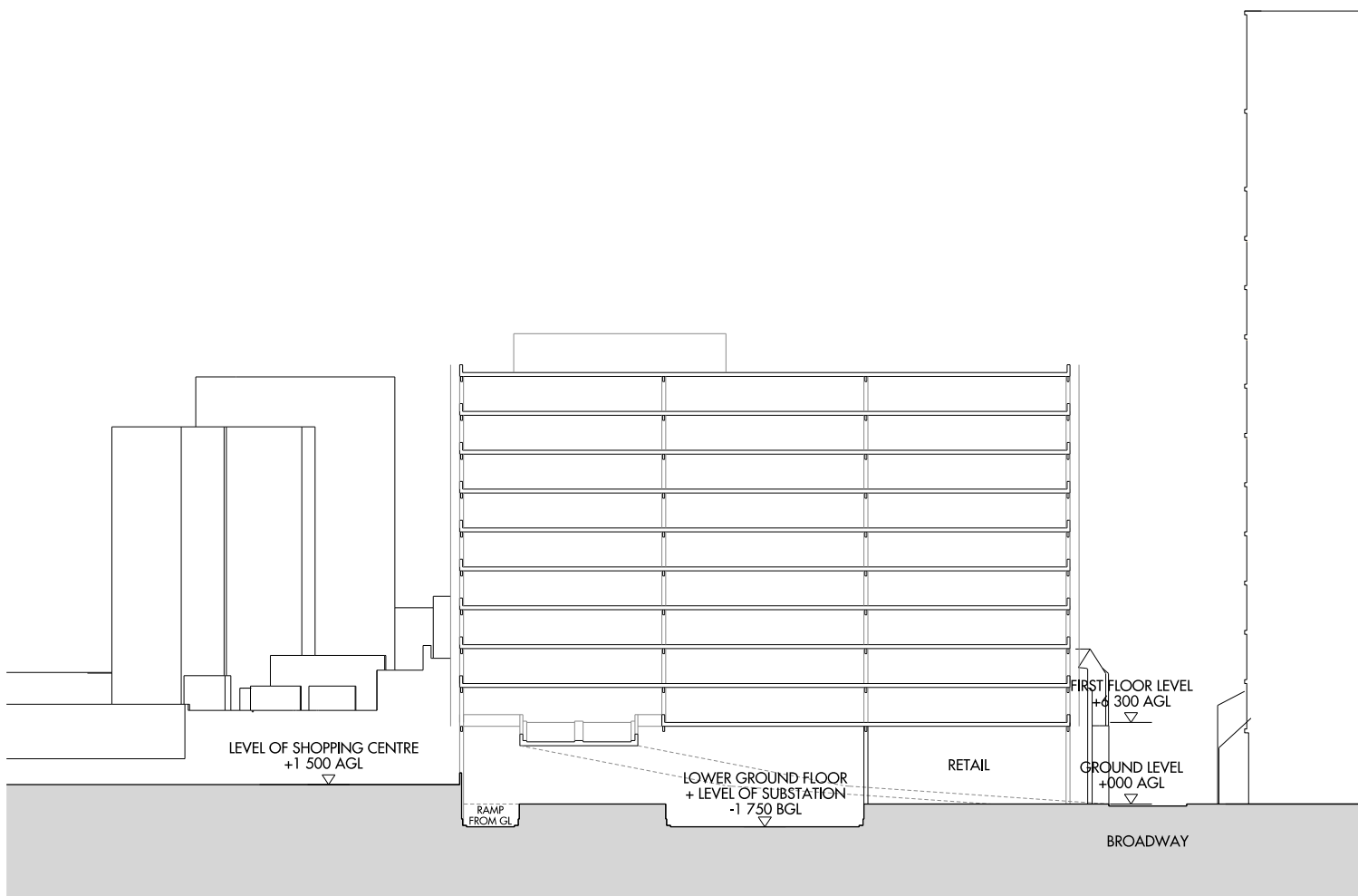
FIRST FLOOR PLAN

- 2no entry barriers/ 3no exit barriers
- Shopmobility service with dedicated entrance barrier, private parking and direct access to the lift core
- Private spaces to the southern aisle



TYPICAL UPPER FLOOR PLAN

- East and west core providing 2no passenger lifts and stairs respectively
- 3no aisles of parking bays plus dedicated accessible parking areas
- Clear, pedestrian routes to the lift and stair cores



SITE SECTION



3.6 KEY DESIGN DATA

1,435NO PARKING BAYS

1ST FLOOR - 67 NO. BAYS

2ND TO 9TH FLOOR - 150 NO. BAYS (PER FLOOR)

TOTAL - 1,435 NO. BAYS

INC. 11% UA/ PARENT & CHILD

C.+100NO MAINTAINING ACCESS WITH ADJOINING NICHOLSON'S SHOPPING CAR PARK

2.5x4.8m GENEROUS BAY SIZE

7.5x15.6m STRUCTURAL GRID PROVIDING COLUMN FREE PARKING

3.0m FLOOR-TO-FLOOR HEIGHT

MIN.2.2m CLEAR HEAD HEIGHT

G+10 STOREYS OF PARKING

LG - 960M² GEA

G - 3,450M² GEA

1ST FLOOR - 4,000M² GEA

2ND TO 9TH FLOOR - 4,320M² GEA

TOTAL AREA - 47,290M² GEA

UP/DOWN SEPARATE UP AND DOWN RAMPS, ALONG WITH ONE-WAY CIRCULATION

AISLES TO MINIMISE VEHICULAR CONFLICTS AND MAXIMISE DYNAMIC CAPACITY

SIGNAGE VARIABLE MESSAGE SIGNAGE SYSTEM (VMS)



3.7 ACCESS, MOVEMENT & TRAFFIC FLOWS

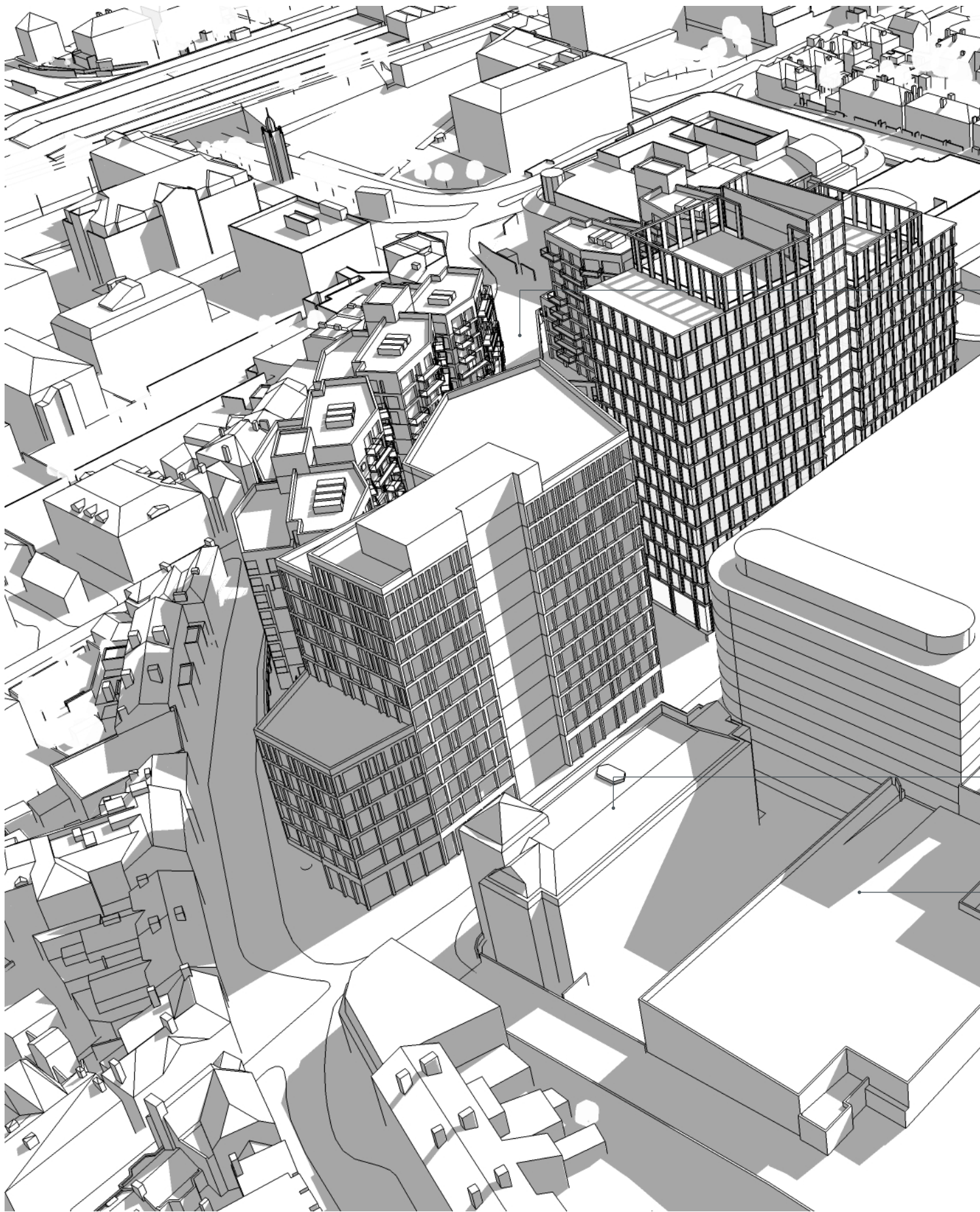
Access/egress to the car park will be via a dedicated entry point served by a new junction on the Broadway. This access will in turn feed into two way ramp up to the first level of parking. This ramp will consist of a 4m wide entry lane, a 4m wide exit lane and a central 0.5m kerbed island. The ramp will be laid to maximum gradient of 10%. Where the ramp turns the inner kerblines radius shall be no less than 4m. A minimum offset of 0.3m from kerblines to parapet wall should also be provided on each side of the ramp.

On arrival at the first level of parking the entry traffic will be required to pass through two dedicated entry barriers. Dedicated shopmobility will also be located on the first level of parking and will have its own dedicated entrance barrier. Exit traffic departing from the first level of parking will also pass through a two barrier exit plaza in order to egress from the car park via the two way ramp described above.

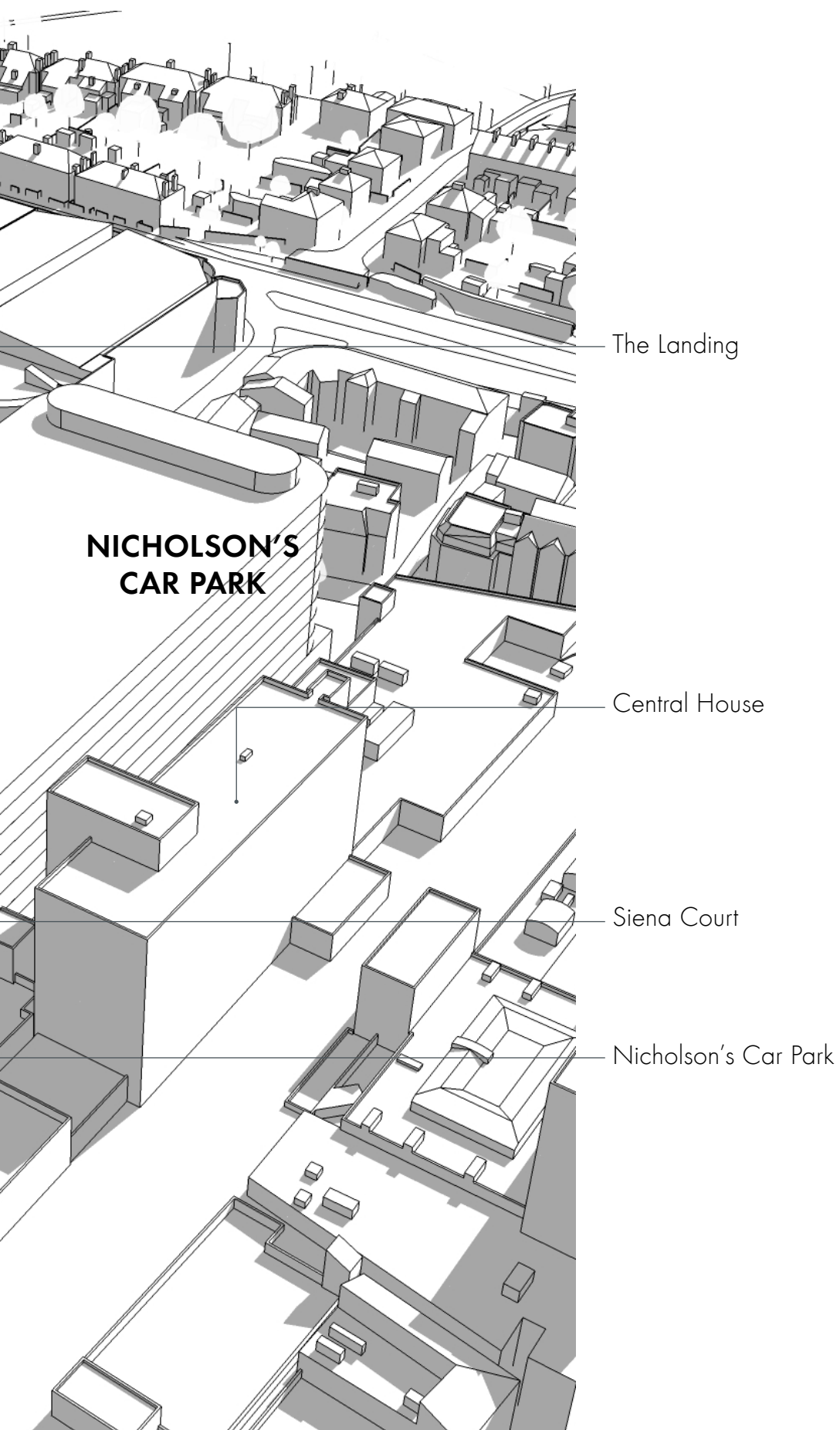
Vertical circulation through the car park will be via a pair of ramps. Separate up and down ramps will be provided and will have a maximum kerb to kerb width of 4m, 0.4m wide side verges to walls

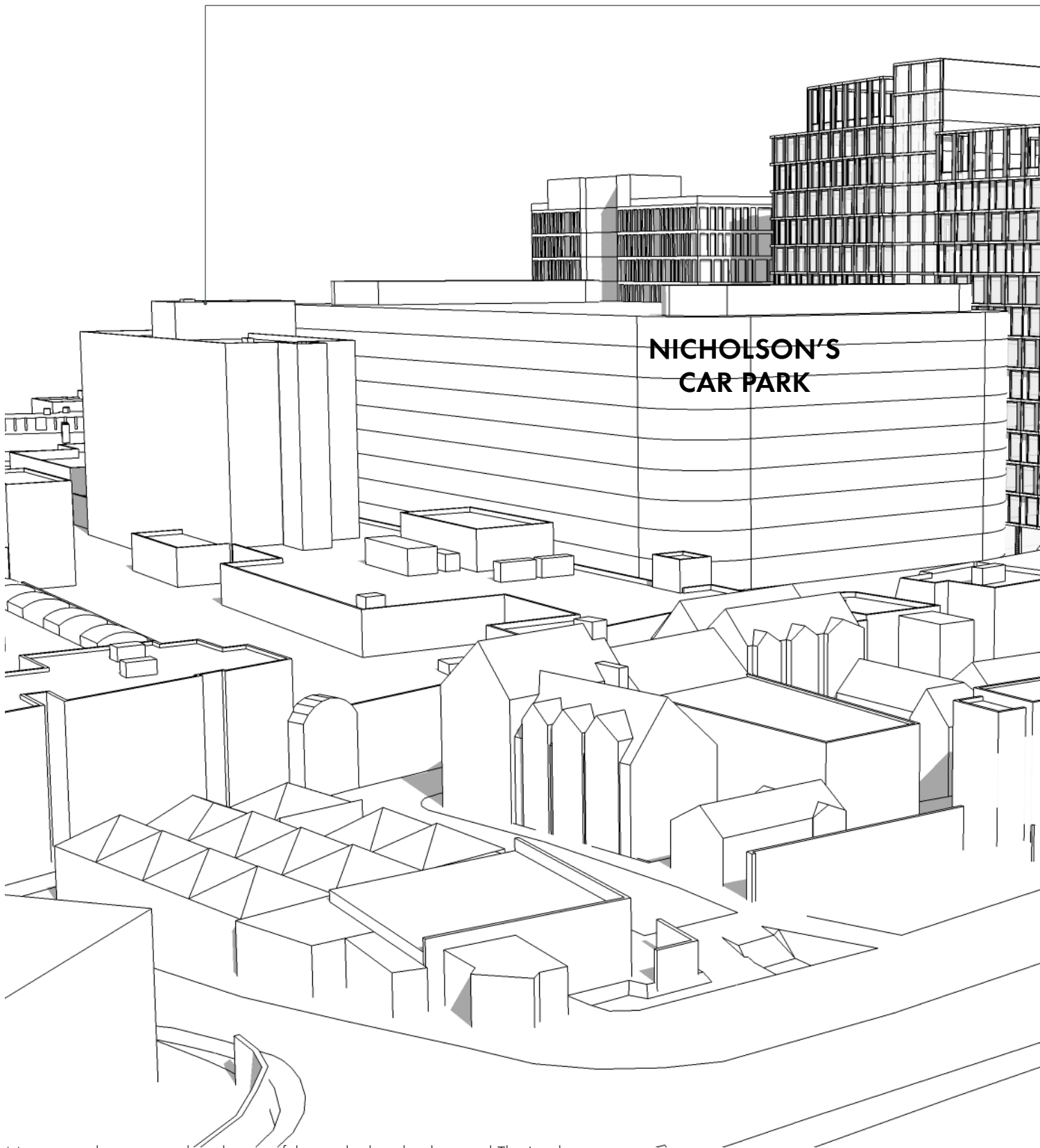
and a 4m minimum inner kerb-line radius. The maximum gradient of the ramps shall be 1 in 10 measured along a line offset 1m from the inner kerb-line. The up ramp will be a "D" shaped helical ramp where the end curves are also sloping at 1 in 10. This enables the ramp to be shorter in plan so that the existing connections to the additional two parking levels to the east can be maintained.

Searching time for a vacant space within the multilevel car park will be reduced by providing an internal Variable Message Signage system (VMS), particularly during busy periods. A soffit mounted intelligent sign face will be located on each floor (9 signs in total) and be visible to drivers as they land on each floor from the up ramp. This sign shall display the number of spaces available on the level that the driver has landed on and also the total number of spaces available on the upper levels combined. This will enable drivers to make an informed decision to either leave the ramp and find a space on a particular level, or remain on the ramp to locate a space on one of the upper levels. Vehicles will be counted onto and off of each level by an induction loop positioned at the start and the end of each ramp.

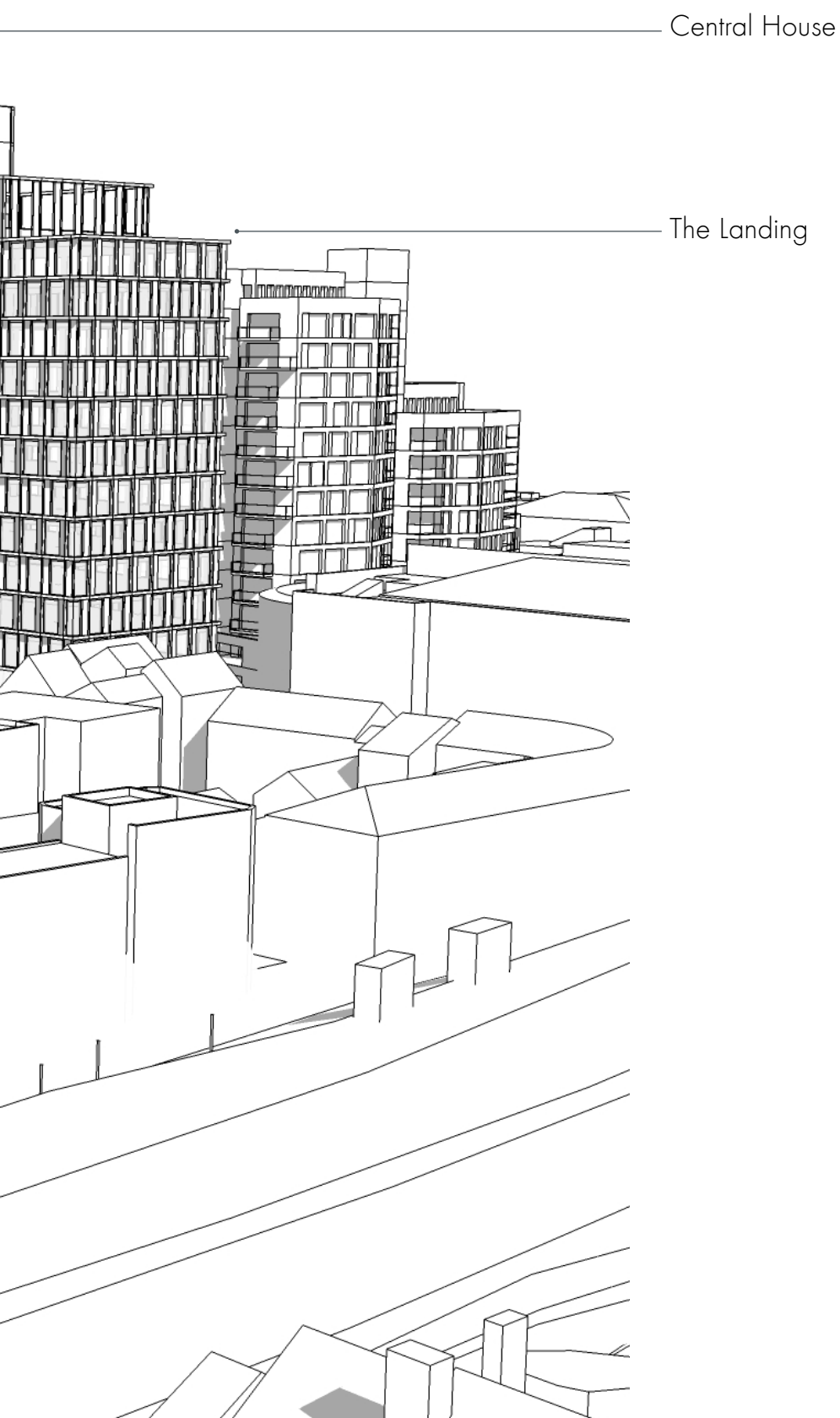


Massing study; as viewed north east of the site looking back toward the Station

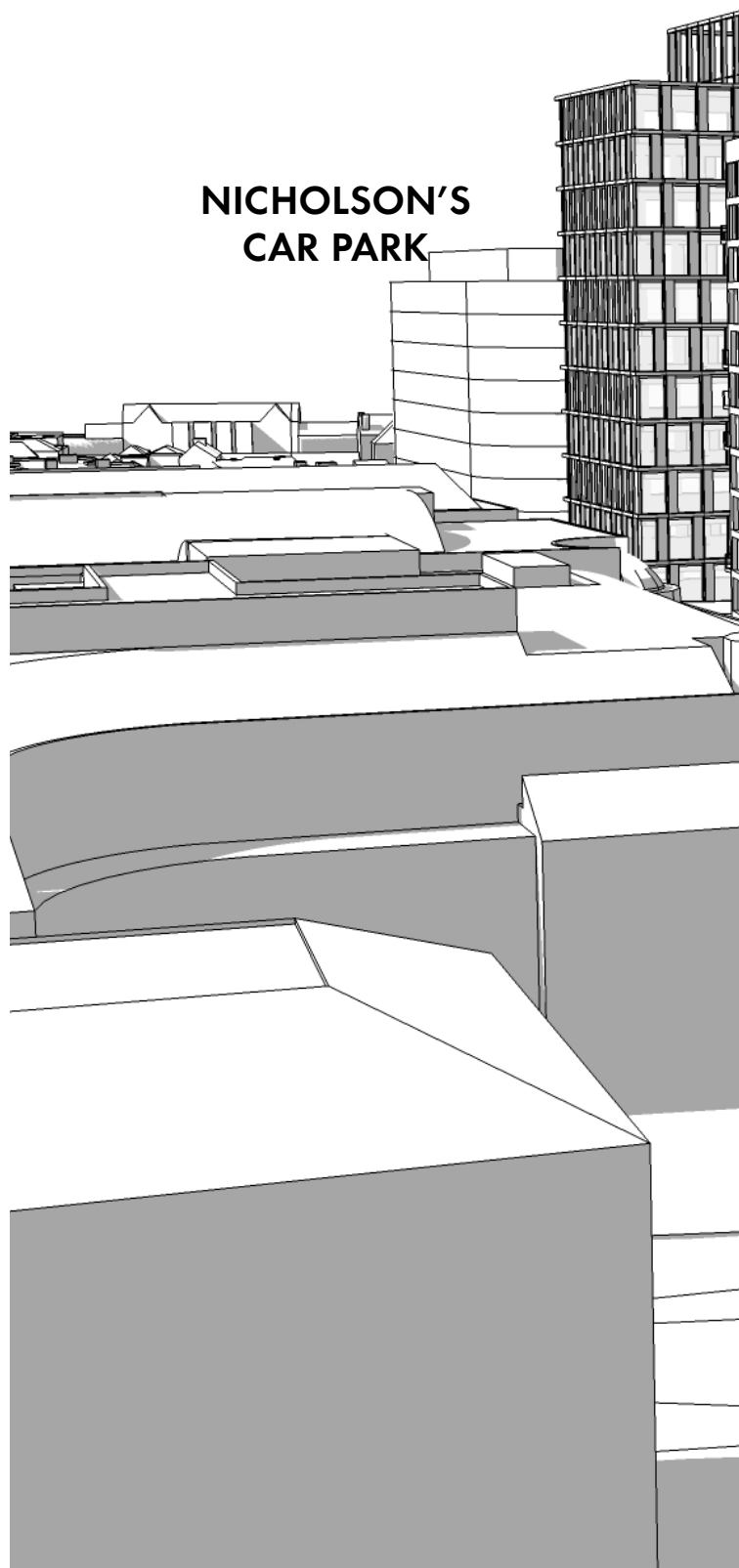




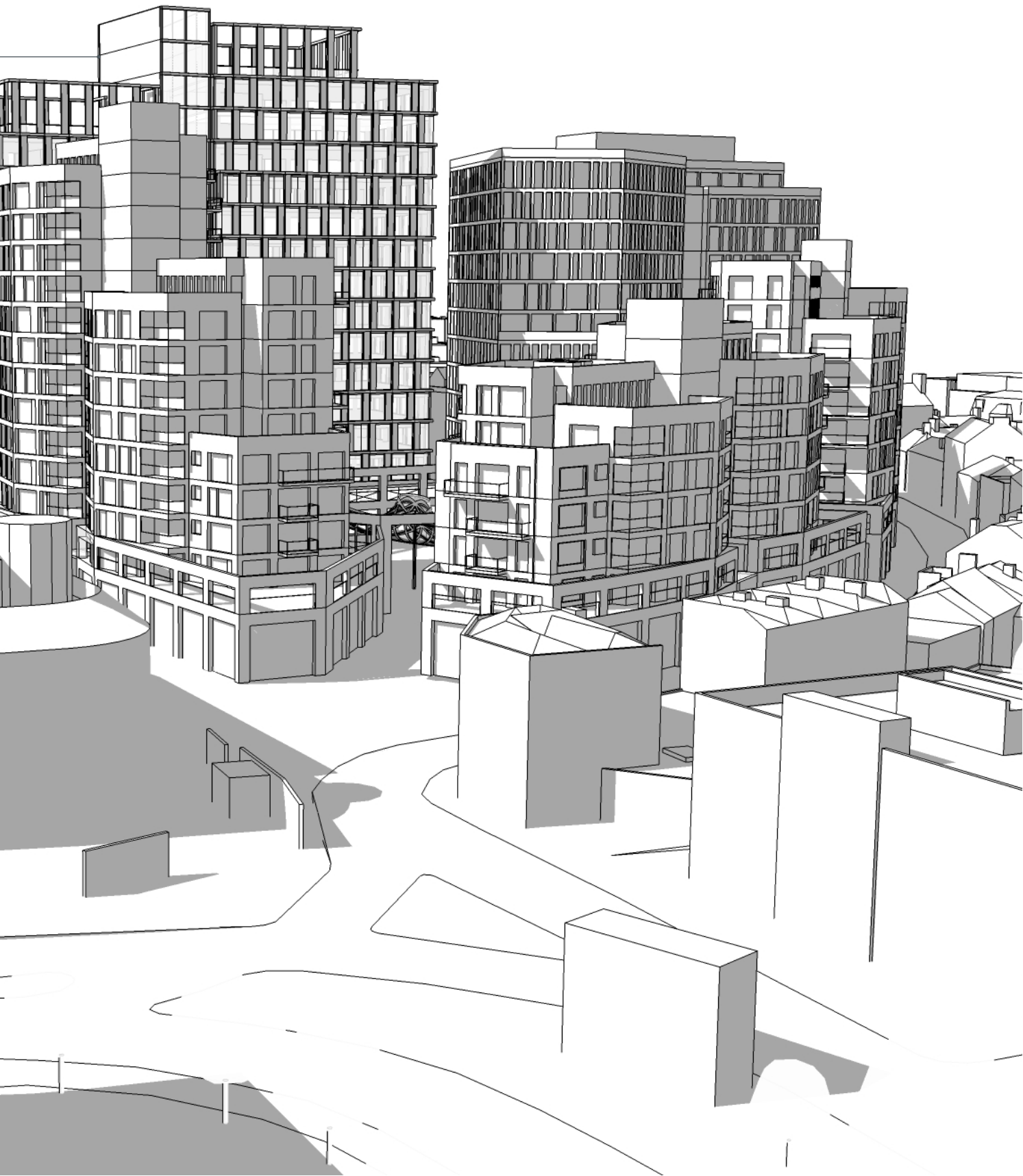
Massing study; as viewed north west of the site looking back toward The Landing



**NICHOLSON'S
CAR PARK**



Massing study; as viewed south west of the site





FACADE DESIGN PRECEDENTS



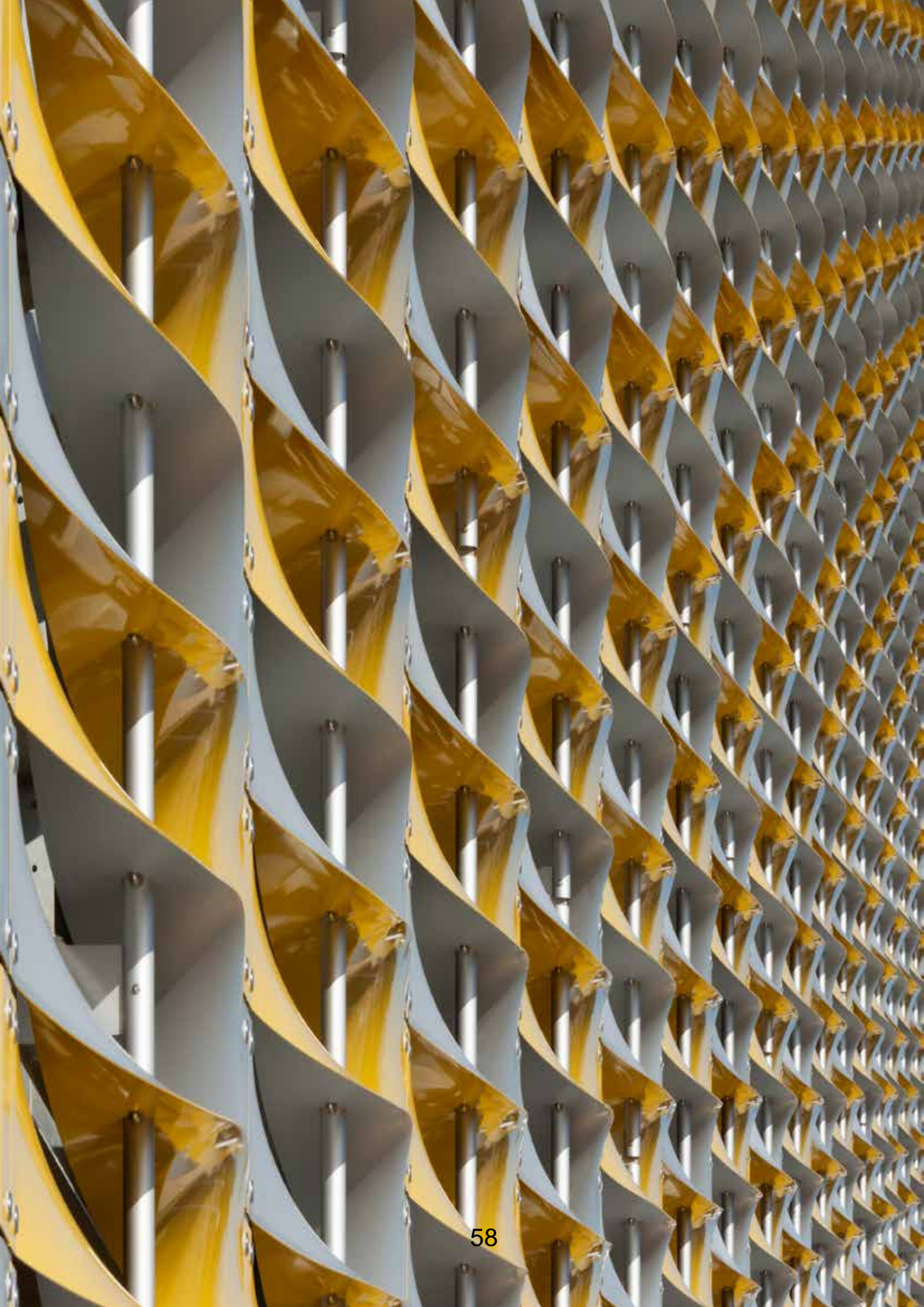
Southmead Hospital Car Park, Bristol

BDP Architects

400 Parking Spaces

Client: North Bristol NHS Trust

Completed: 2016



FACADE DESIGN PRECEDENTS



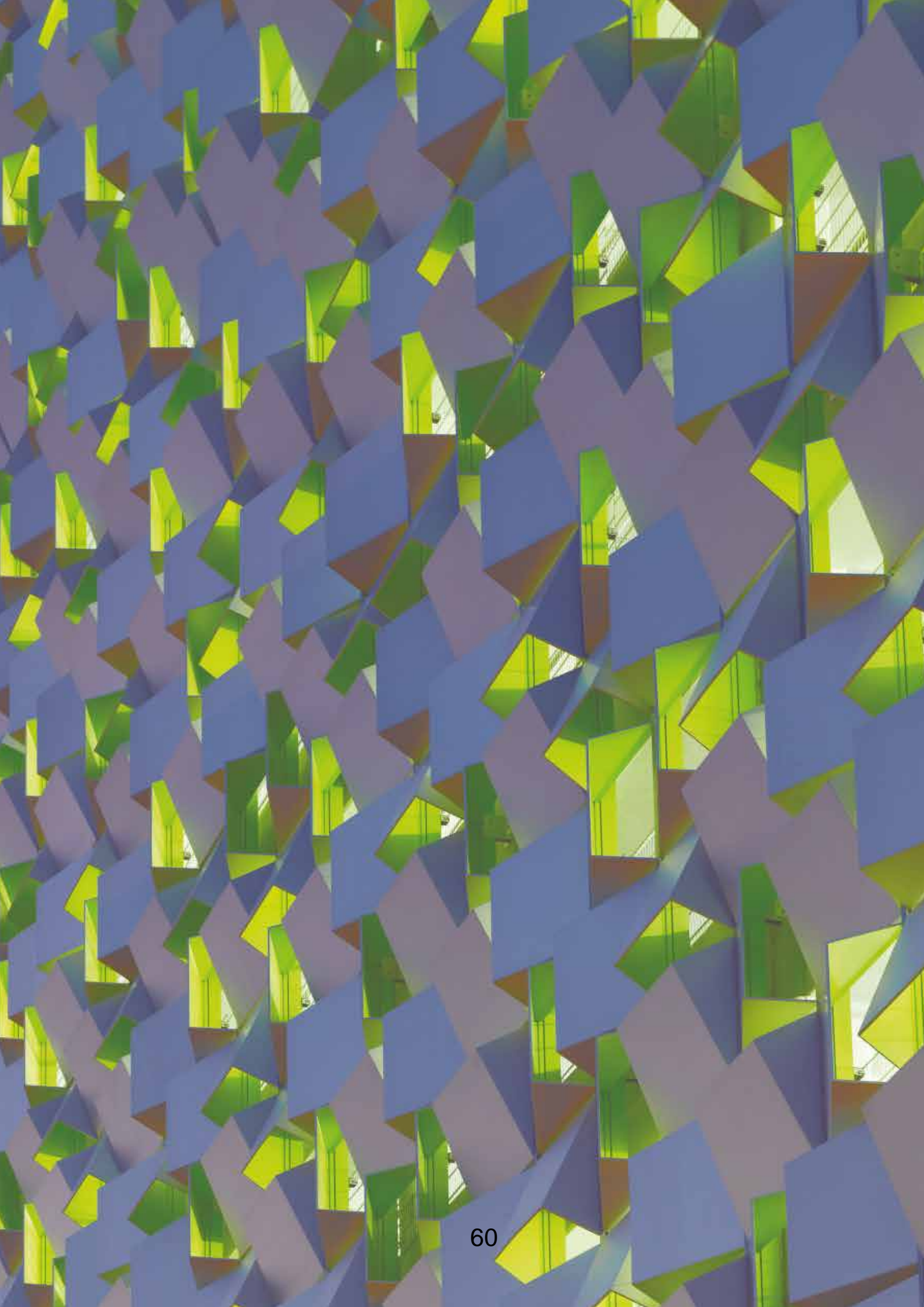
Addebbrooke's Car Park, Cambridge

1228 Parking Spaces

Client: Addenbrooke's Hospital Biomedical Campus

Completed: 2014

Awards: RIBA Award 2015



FACADE DESIGN PRECEDENTS



Charles Street Car Park, Sheffield

520 Parking Spaces

Client: CTP St James Ltd

Completed: 2014

Awards: RIBA Award 2009, RIBA White Rose Awards - Bronze 2009

Allies and Morrison

85 Southwark Street
London SE1 0HX

telephone +44 20 7921 0100
web alliesandmorrison.com
email info@alliesandmorrison.com